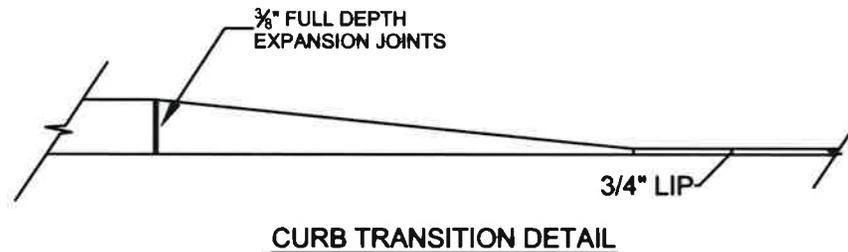
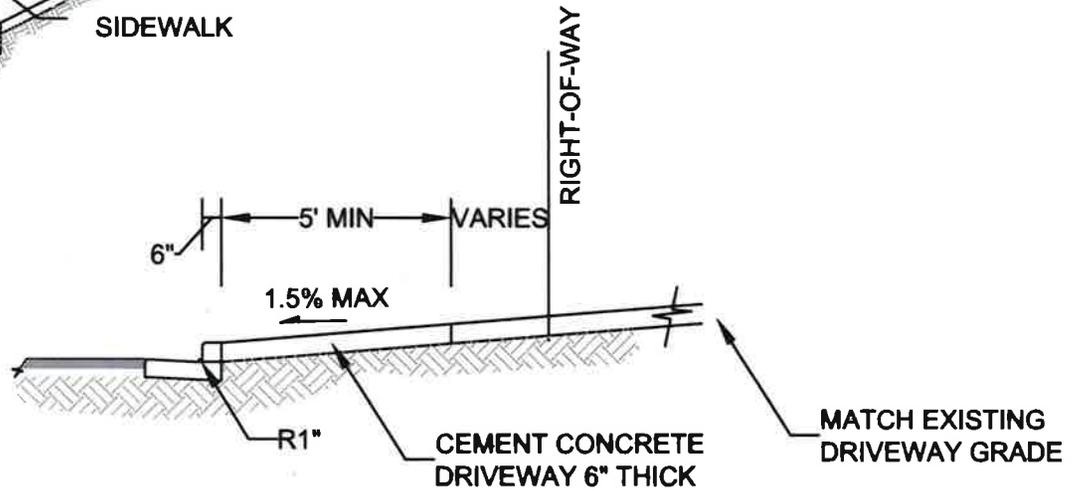
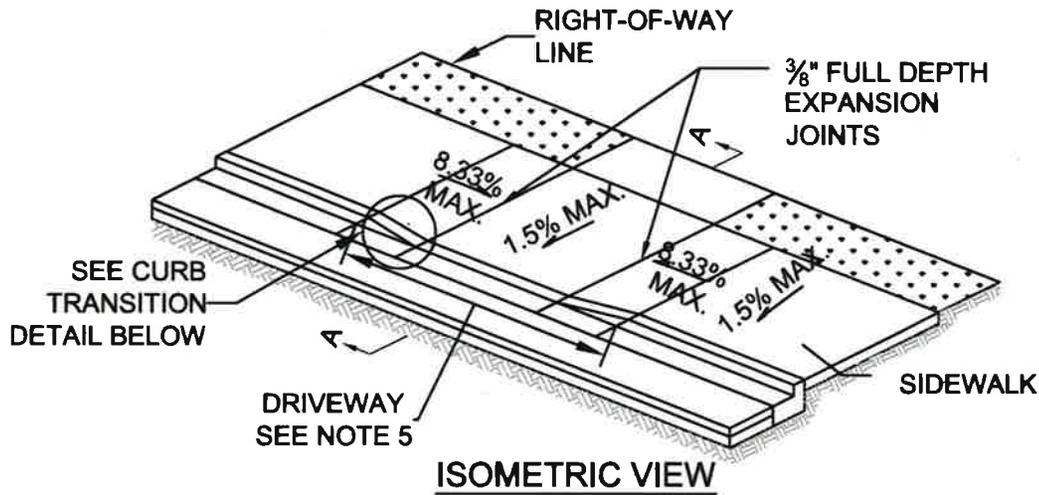


NOTES:

1. ALL COMMERCIAL/INDUSTRIAL DRIVEWAYS SHALL HAVE AN EXPANSION JOINT LOCATED MID-WIDTH.
2. MATERIAL - WSDOT CONCRETE CLASS 4000psi.
3. FULL DEPTH EXPANSION JOINT SHALL BE INSTALLED IF DRIVEWAY WIDTH IS 10' OR GREATER.
4. CURB SHALL BE IN COMPLIANCE WITH STND DWG 312.
5. NO REBAR SHALL BE PLACED IN CURB, GUTTER, DRIVEWAY, OR SIDEWALK.



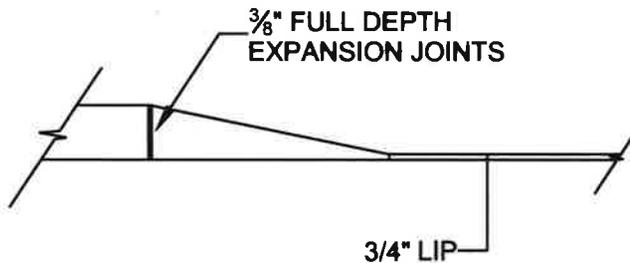
<h2 style="margin: 0;">301</h2> <h3 style="margin: 0;">Curb & Gutter Section Driveway Approach</h3>	
<p style="font-size: small; margin: 0;">CITY OF SHORELINE</p>	
<p style="font-size: small; margin: 0;">Public Works</p>	
<p style="font-size: x-small; margin: 0;">NOT TO SCALE</p>	<p style="font-size: x-small; margin: 0;">Revision Date April 2017</p>



SECTION A-A

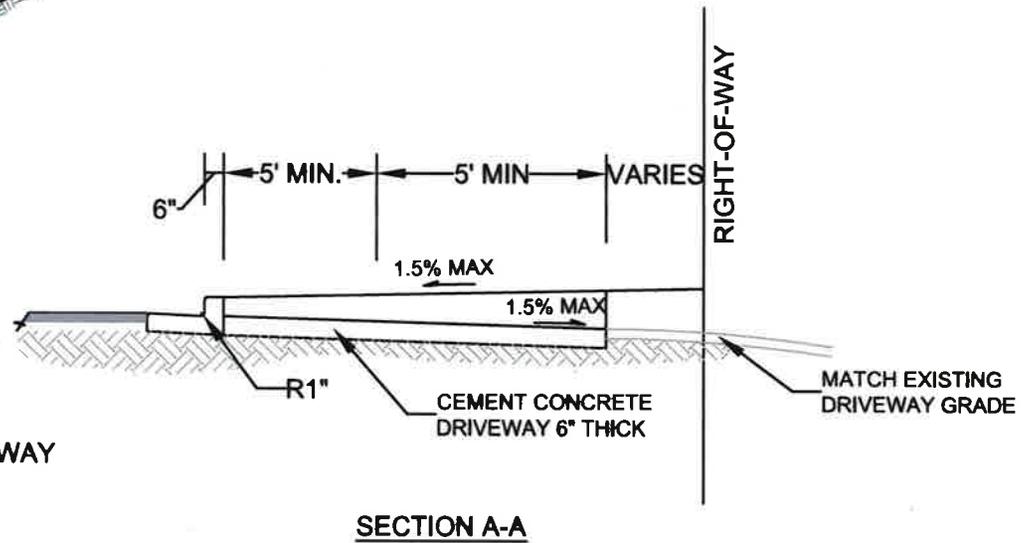
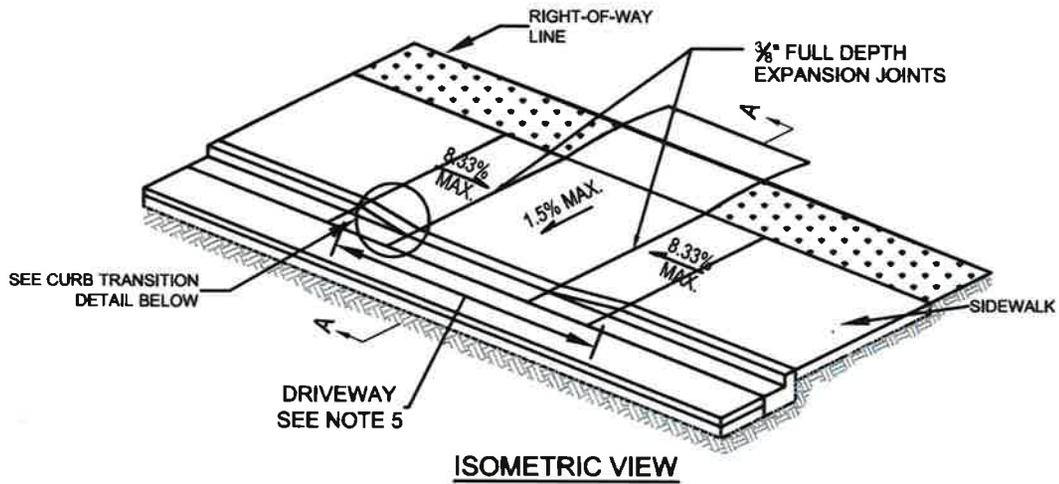
NOTES:

1. MATERIAL - WSDOT CONCRETE CLASS 4000psi.
2. FULL DEPTH EXPANSION JOINT SHALL BE INSTALLED IF DRIVEWAY WIDTH IS 10' OR GREATER.
3. CURB SHALL BE IN COMPLIANCE WITH STND DWG 312.
4. NO REBAR SHALL BE PLACED IN CURB, GUTTER, DRIVEWAY, OR SIDEWALK.
5. DRIVEWAY HAS TO BE DESIGNED TO MEET ADA.



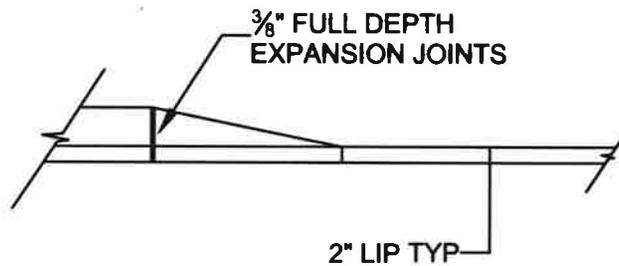
CURB TRANSITION DETAIL

302	
Driveway Approach without Amenity Zone (5' Sidewalk Width)	
 CITY OF SHORELINE Public Works	
NOT TO SCALE	Revision Date April 2017

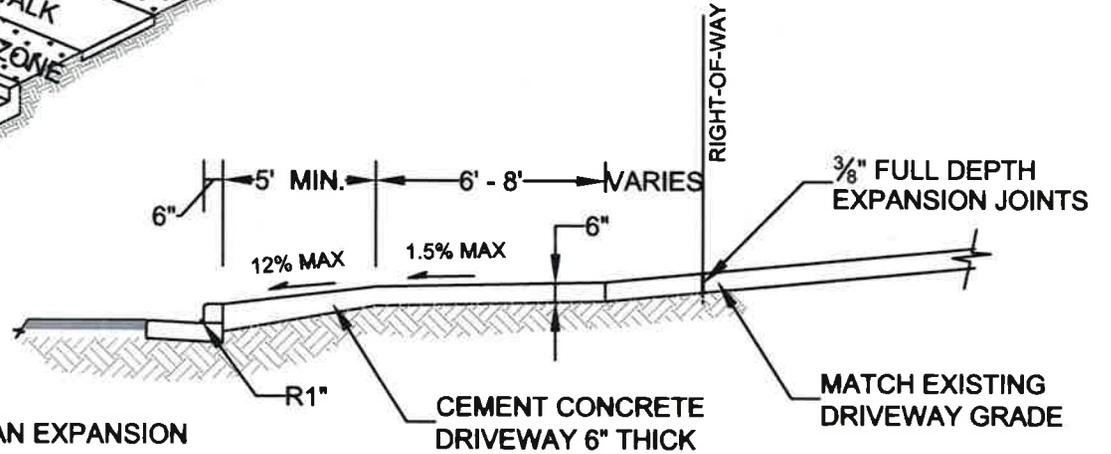
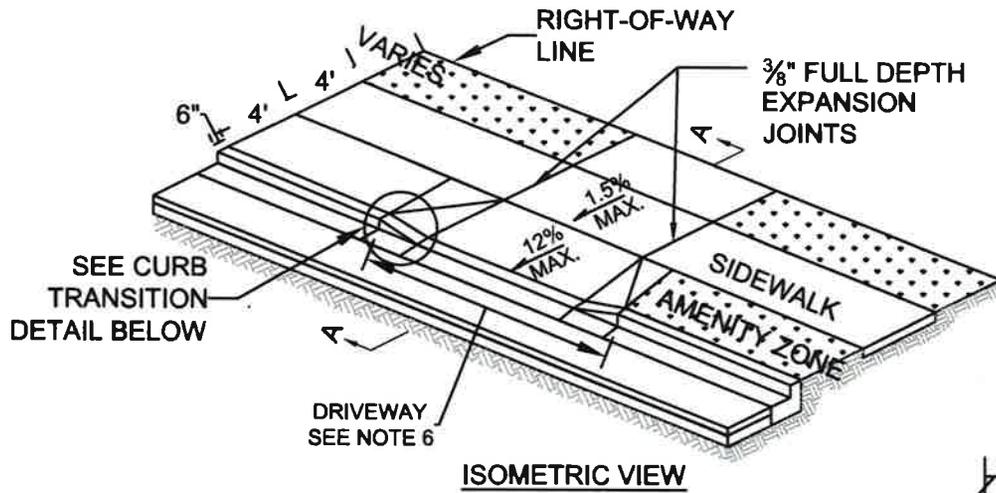


NOTES:

1. MATERIAL - WSDOT CONCRETE CLASS 4000psi.
2. FULL DEPTH EXPANSION JOINT SHALL BE INSTALLED IF DRIVEWAY WIDTH IS 10' OR GREATER.
3. CURB SHALL BE IN COMPLIANCE WITH STND DWG 312.
4. NO REBAR SHALL BE PLACED IN CURB, GUTTER, DRIVEWAY, OR SIDEWALK.
5. DRIVEWAY HAS TO BE DESIGNED TO MEET ADA.



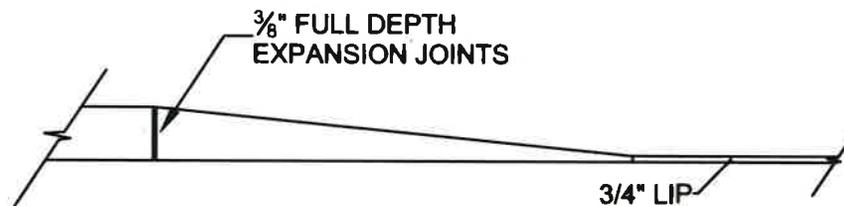
<h1 style="margin: 0;">304</h1> <h2 style="margin: 0;">Driveway Approach: Reverse Slope without Amenity Zone</h2>	
 CITY OF SHORELINE Public Works	 TRACY J. TURNER PROFESSIONAL ENGINEER
NOT TO SCALE	Revision Date April 2017



NOTES:

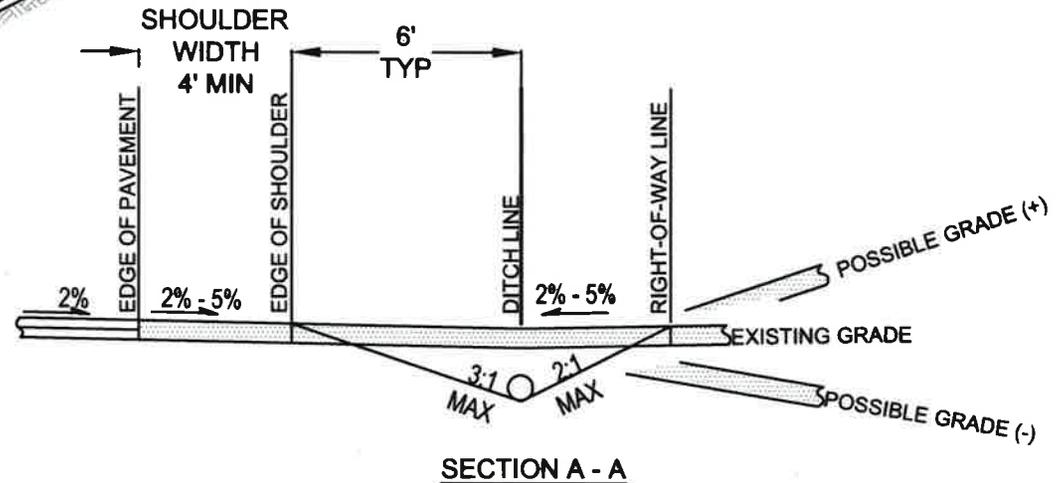
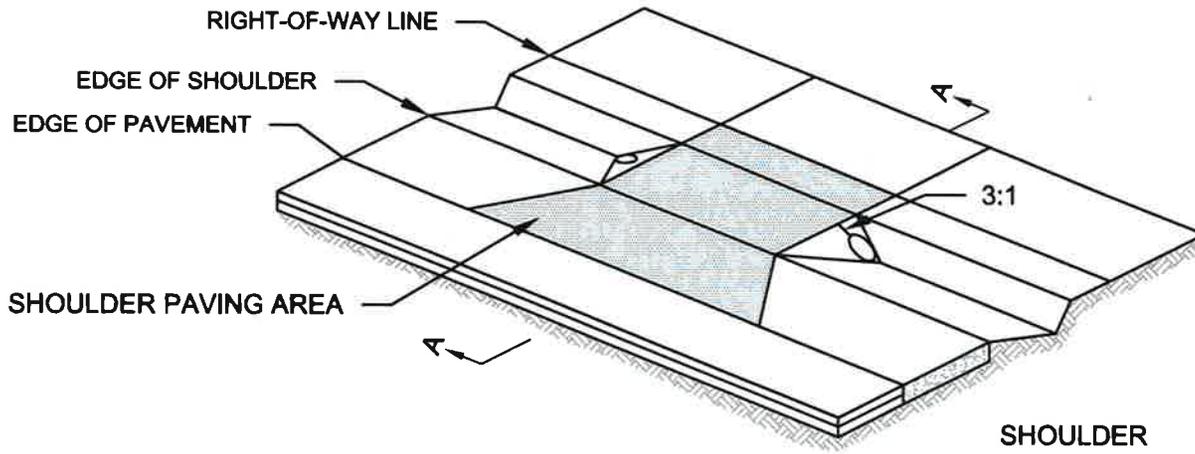
1. ALL COMMERCIAL/INDUSTRIAL DRIVEWAYS SHALL HAVE AN EXPANSION JOINT LOCATED MID-WIDTH.
2. MATERIAL - WSDOT CONCRETE CLASS 4000psi.
3. FULL DEPTH EXPANSION JOINT SHALL BE INSTALLED IF DRIVEWAY WIDTH IS 10' OR GREATER.
4. CURB SHALL BE IN COMPLIANCE WITH STND DWG 312.
5. NO REBAR SHALL BE PLACED IN CURB, GUTTER, DRIVEWAY, OR SIDEWALK.
6. DRIVEWAY HAS TO BE DESIGNED TO MEET ADA.

SECTION A-A



CURB TRANSITION DETAIL

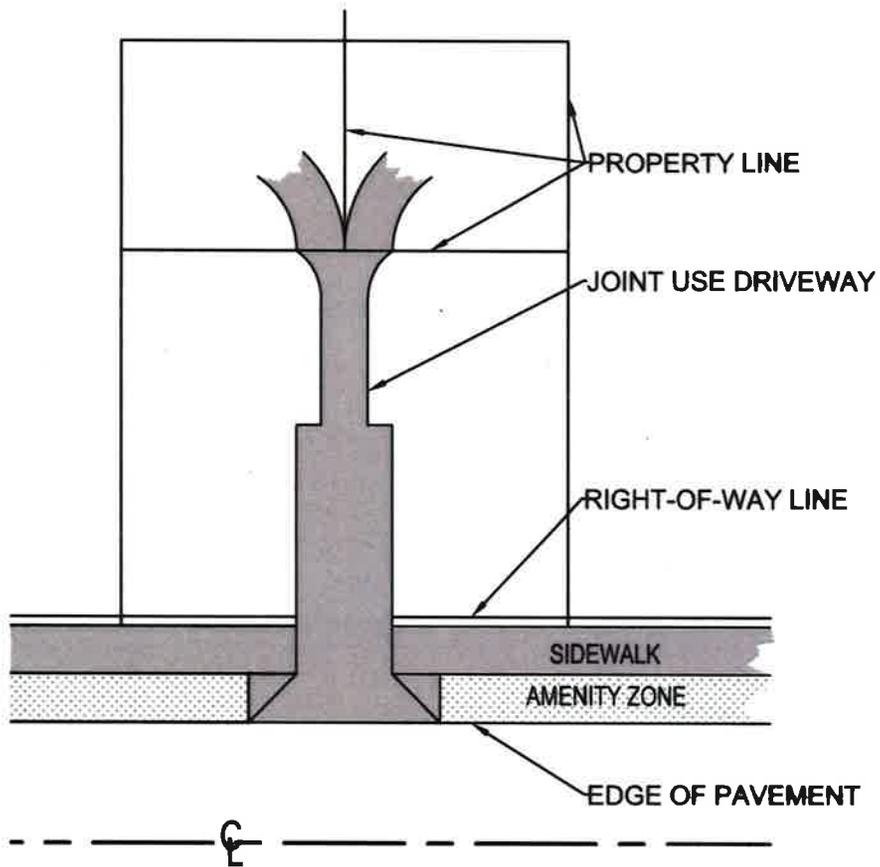
<p>305 Driveway Approach without Amenity Zone (8' Sidewalk Width)</p>	
 CITY OF SHORELINE Public Works	
NOT TO SCALE	Revision Date April 2017



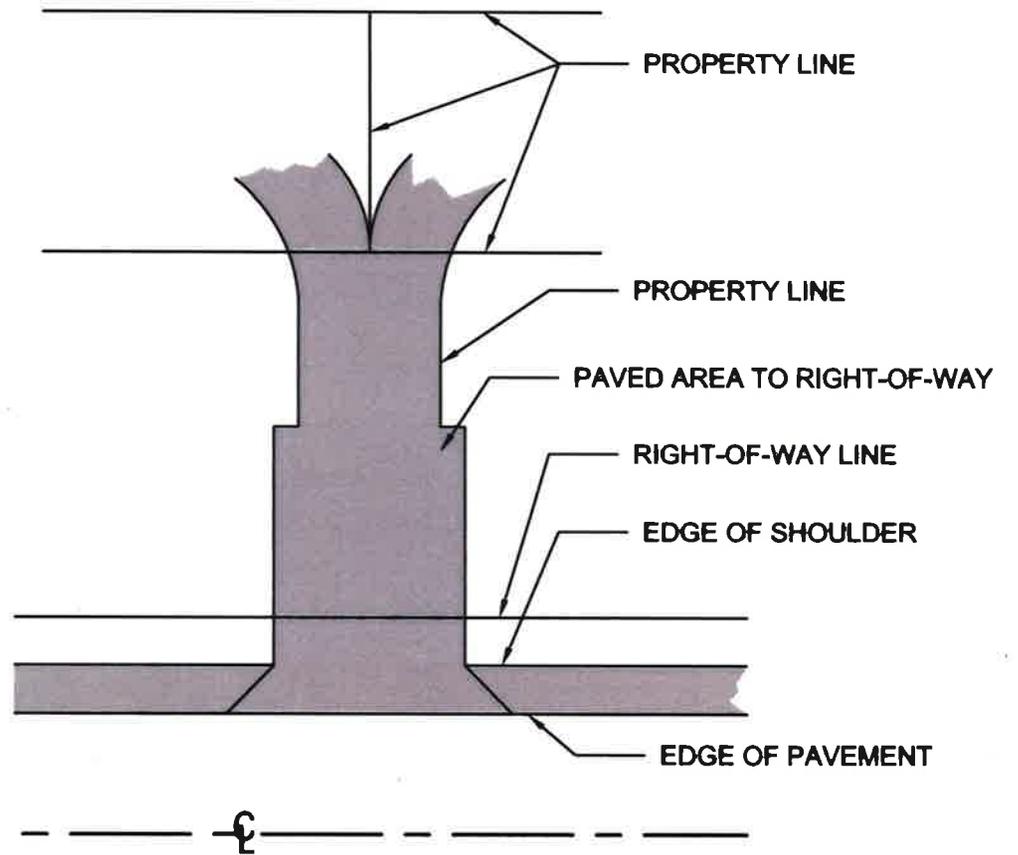
NOTES:

1. ALL COMMERCIAL/INDUSTRIAL DRIVEWAYS SHALL HAVE AN EXPANSION JOINT LOCATED MID-WIDTH.
2. PIPE SHALL BE:
 - A. SIZED TO CONVEY COMPUTED STORM WATER RUNOFF, AND
 - B. MIN. 12" DIAM., AND
 - C. EQUAL TO OR LARGER THAN EXISTING PIPES WITHIN 500' UPSTREAM.
3. EXPOSED PIPE ENDS SHALL BE BEVELED TO MATCH THE SLOPE FACE AND PROJECT NO MORE THAN 2" BEYOND SLOPE SURFACE. PROJECTING HEADWALLS ARE NOT ACCEPTABLE.
4. PIPE COVER SHALL BE IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS.
5. PIPE SHALL BE INSTALLED IN A STRAIGHT UNIFORM ALIGNMENT AT A MIN. 0.5% SLOPE (0.5 FT. PER 100 FT.)
6. DRIVEWAYS SHALL BE PAVED FROM THE EDGE OF PAVEMENT TO THE PROPERTY LINE.

<h2 style="margin: 0;">306</h2> <h3 style="margin: 0;">Shoulder & Ditch Section Driveway</h3>	
 CITY OF SHORELINE Public Works	
NOT TO SCALE	Revision Date April 2017



WITH CURB



WITH NO CURB

307 Joint Use Driveway

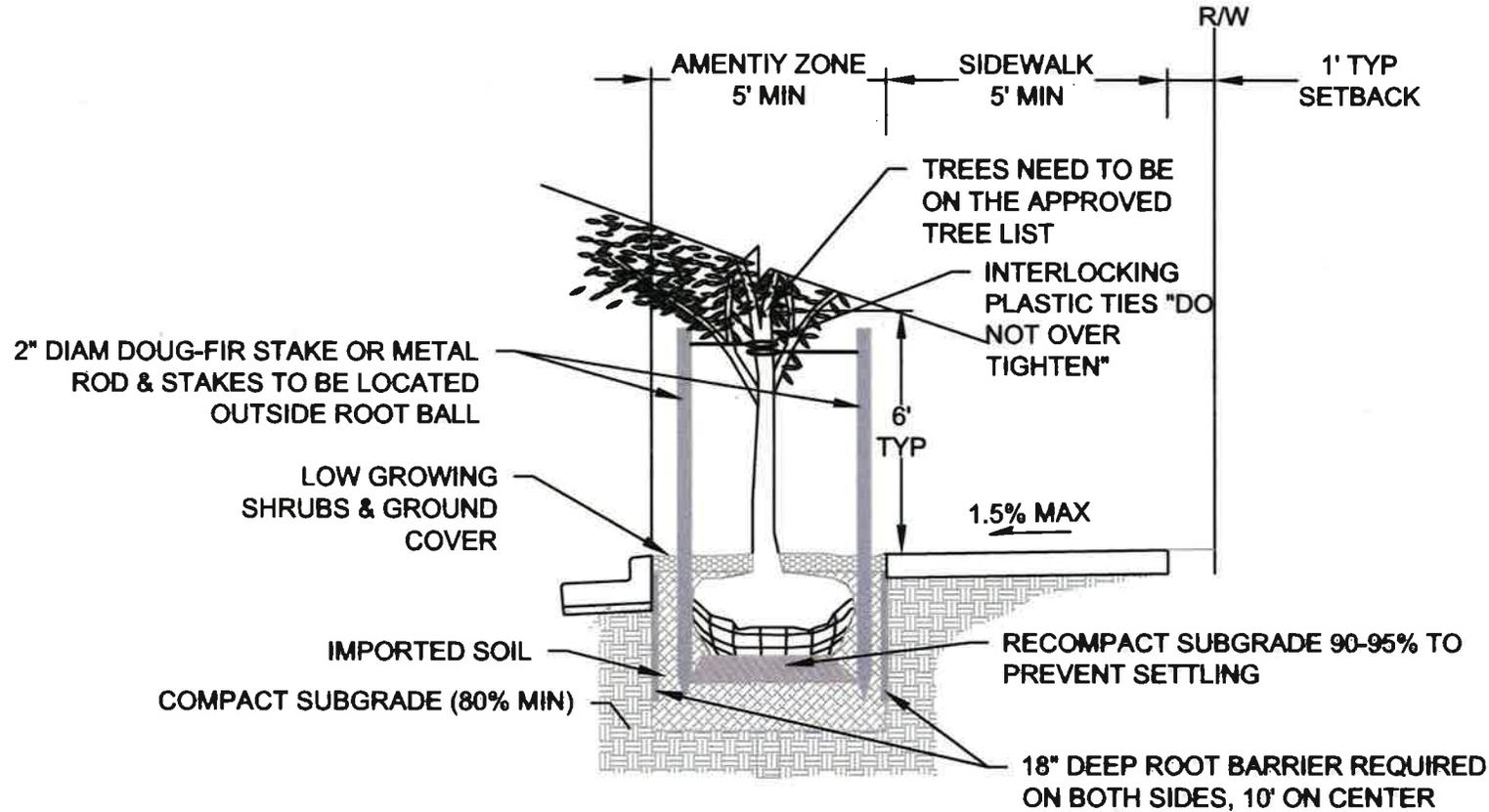


Public Works



NOT TO SCALE

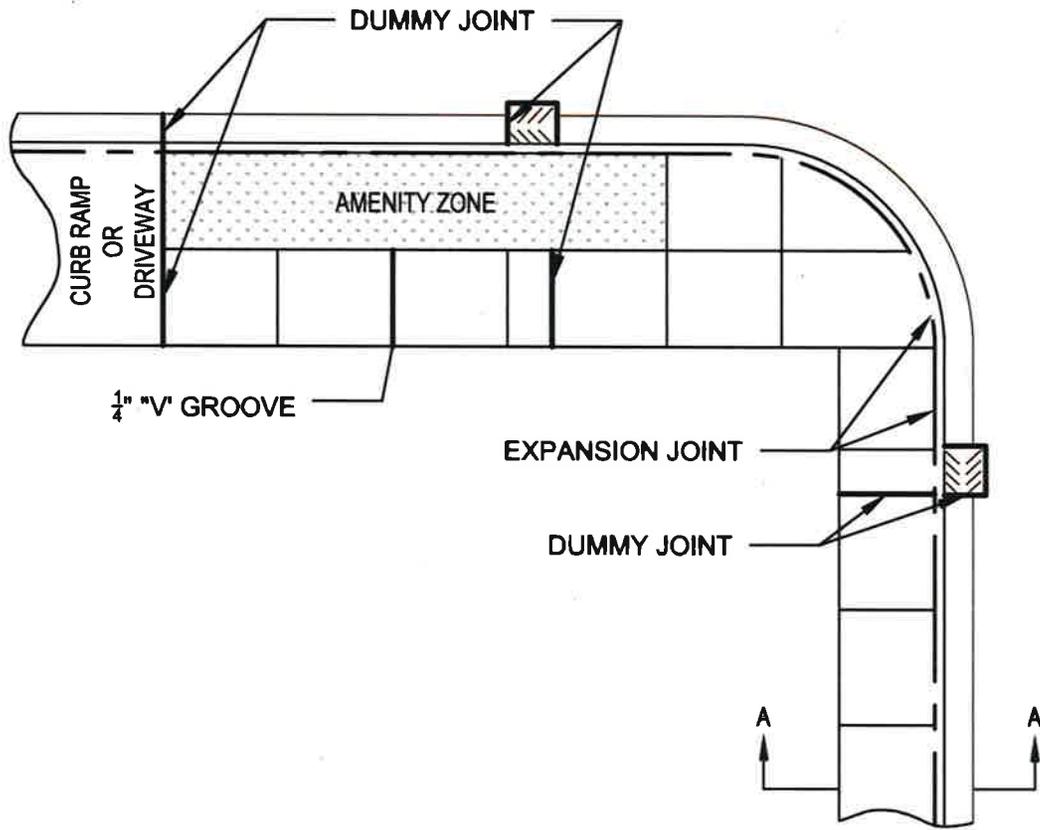
Revision Date
April 2017



NOTES:

1. TREE PIT SHALL NOT BE LESS THAN 2 TIMES ROOT BALL DIAM.
2. CUT ALL TIES & FOLD BACK BURLAP FROM UPPER $\frac{1}{3}$ OF ROOT BALL.
3. WATER DAILY UNTIL ESTABLISHED, FERTILIZE & USE GROWTH HORMONE.
4. WHERE A CONTINUOUS PLANTING STRIP IS ALLOWED, WIDEN TREE PIT TO SIDEWALK.
5. IF TREE IS REMOVED FOR RIGHT-OF-WAY, A TREE NEED TO BE PLANTED.
6. TREE SPACING PER PLAN & FILED APPROVAL BY THE ENGINEER (CITY OF SEATTLE, 100c).

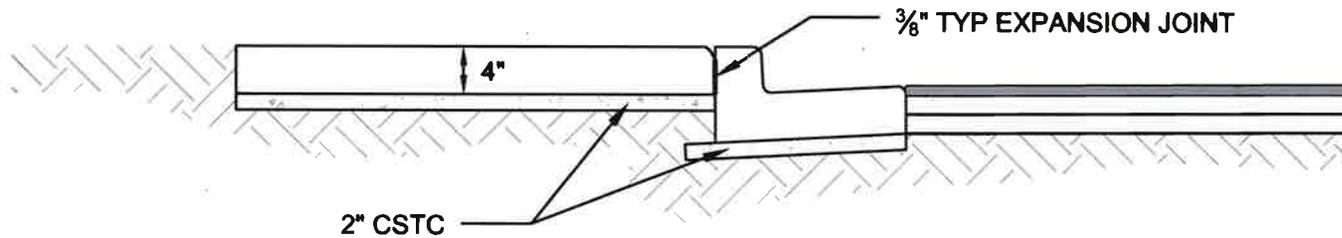
<h2 style="margin: 0;">308</h2> <h3 style="margin: 0;">Street Tree</h3>	
<p style="margin: 0;">CITY OF SHORELINE</p> <p style="margin: 0;">Public Works</p>	
<p style="margin: 0;">NOT TO SCALE</p>	<p style="margin: 0;">Revision Date April 2017</p>



- DUMMY JOINT @ MAX. 15' C-C, 10' TYP, 15' MAX
- - - - - EXPANSION JOINT ALONG CURB
- 1/4" "V" GROOVE @ MAX. 5' C-C

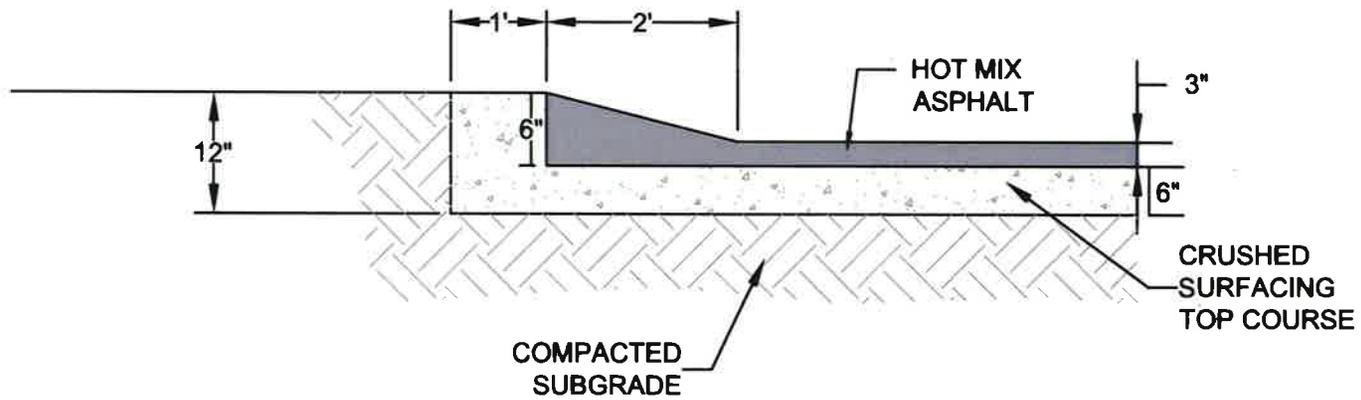
- NOTES:**
1. CONCRETE SHALL HAVE 4" PERIMETER EDGING.
 2. SIDEWALK JOINTS SHOULD MATCH EXPANSION JOINTS.

VERTICAL CURB & SIDEWALK



SECTION A-A

309 Curb & Sidewalk Joints	
 CITY OF SHORELINE Public Works	
NOT TO SCALE	Revision Date April 2017



310
Asphalt Thickened
Edge



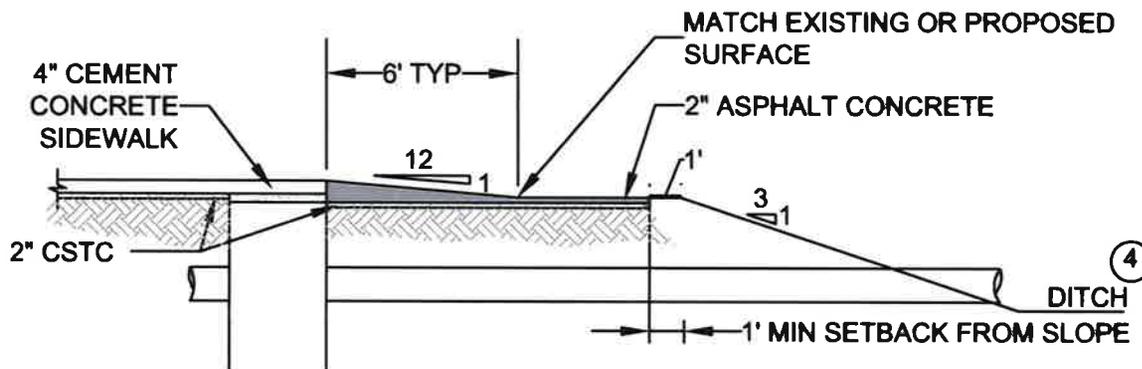
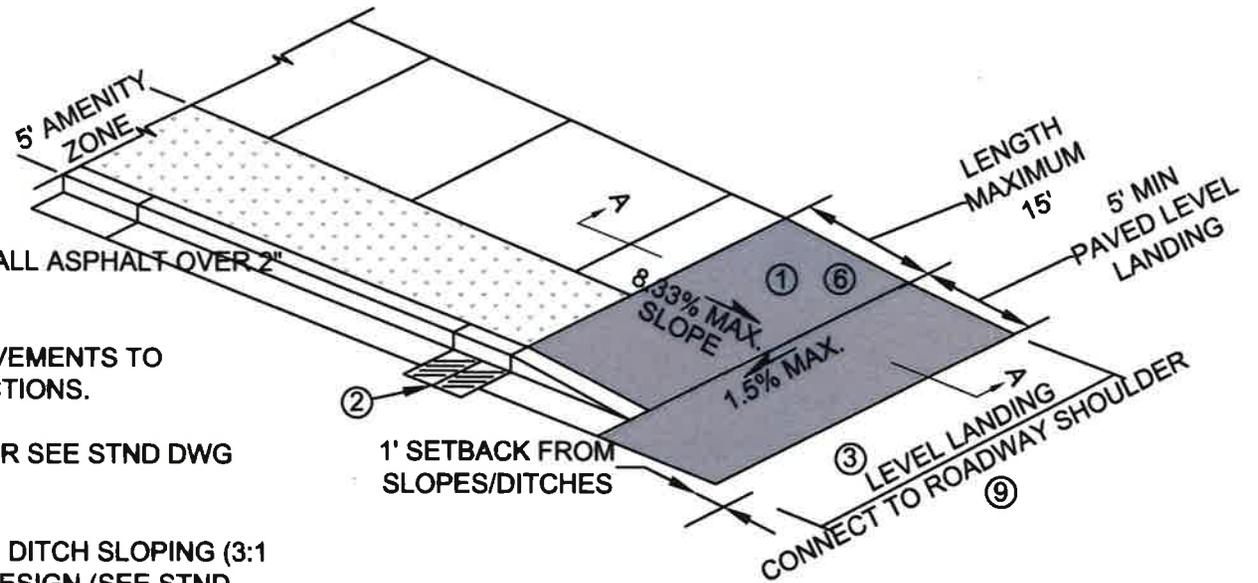
CITY OF SHORELINE
Public Works



NOT TO SCALE	Revision Date April 2017
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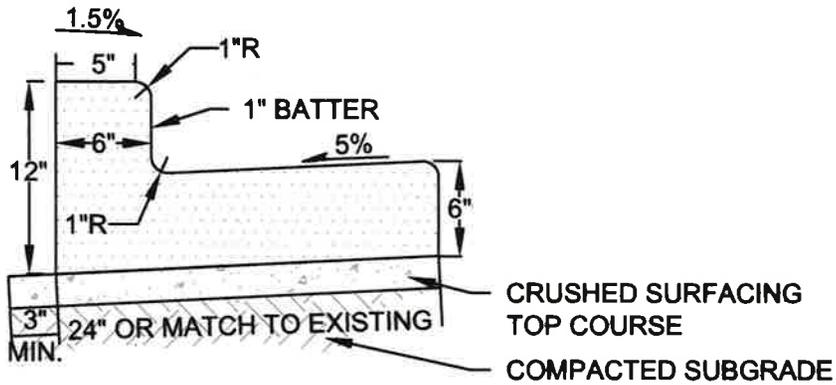
NOTES:

- ① RAMP, CURB TAPER, AND LEVEL LANDING ALL ASPHALT OVER 2" CSTC.
- ② CATCH BASINS LOCATE AT END OF IMPROVEMENTS TO FACILITATE FUTURE EXTENSIONS/CONNECTIONS.
- ③ FOR WIDTHS OF PAVEMENT AND SHOULDER SEE STND DWG 201, 204 & 205.
- ④ FOR TRANSITIONS ADJACENT TO DITCHES, DITCH SLOPING (3:1 TYPICAL), 1' SETBACK & PIPE BEVEL END DESIGN (SEE STND DWG 701) MAY BE REQUIRED.
5. FOR CURB AND SIDEWALK JOINTS SEE STND DWG 309.
- ⑥ DETECTABLE WARNING PATTERN (TRUNCATED DOMES) MAY BE REQUIRED IF RAMP CROSSES TRAVEL LANE OF PUBLIC STREET OR AS SPECIFIED BY THE CITY TRAFFIC ENGINEER (SEE STND DWG 318).
7. CURB RADIUS REQUIRED AT INTERSECTIONS.
- ⑧ EXISTING SHOULDER MAY REQUIRE RESTORATION WITH CSTC.

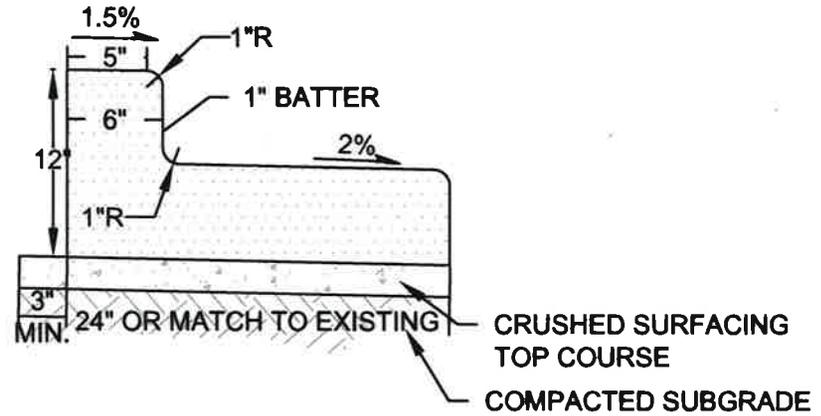


SECTION A-A

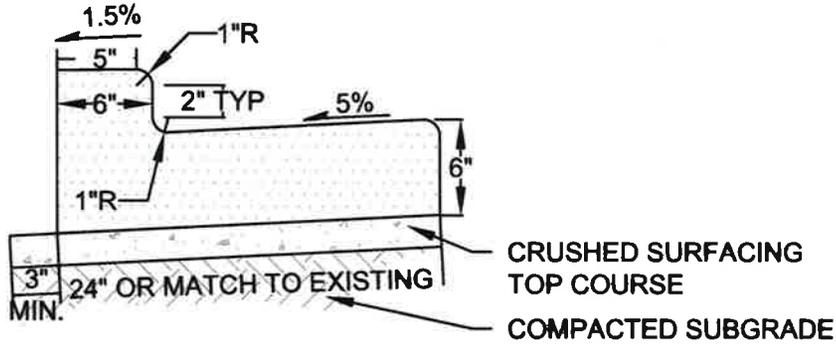
<h2 style="margin: 0;">311</h2> <h3 style="margin: 0;">Asphalt Transition Ramp to Shoulder</h3>	
<p>CITY OF SHORELINE Public Works</p>	
<p>NOT TO SCALE</p>	<p>Revision Date April 2017</p>



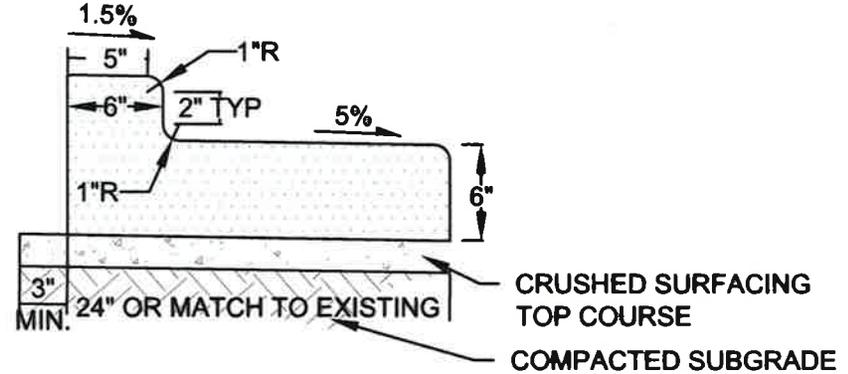
TYPE A CURB AND GUTTER



MEDIAN CURB AND GUTTER

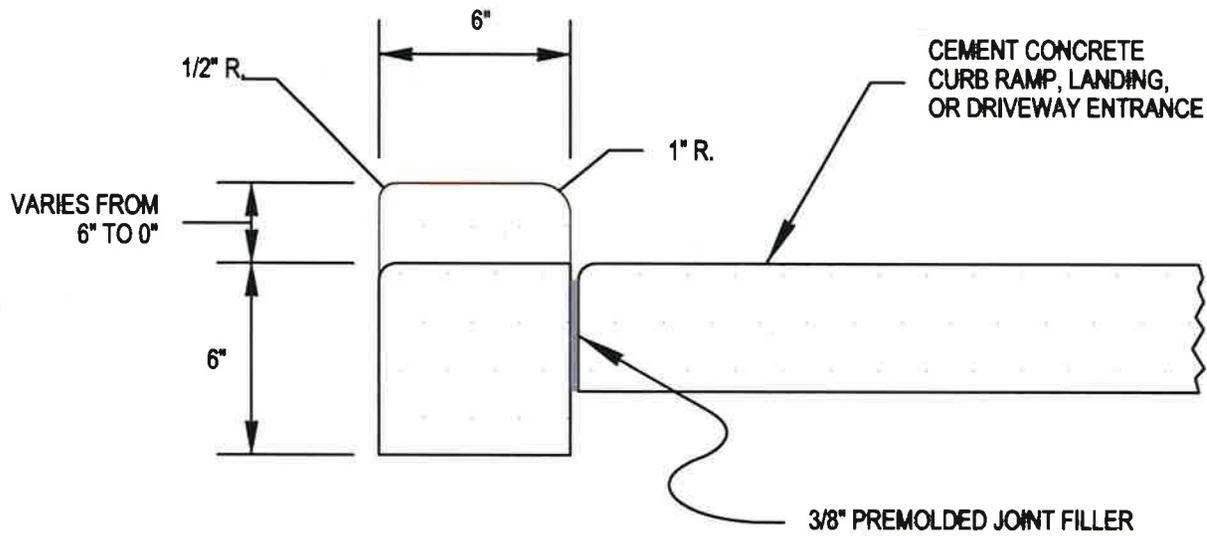


REVERSE DRIVEWAY CURB AND GUTTER (A)

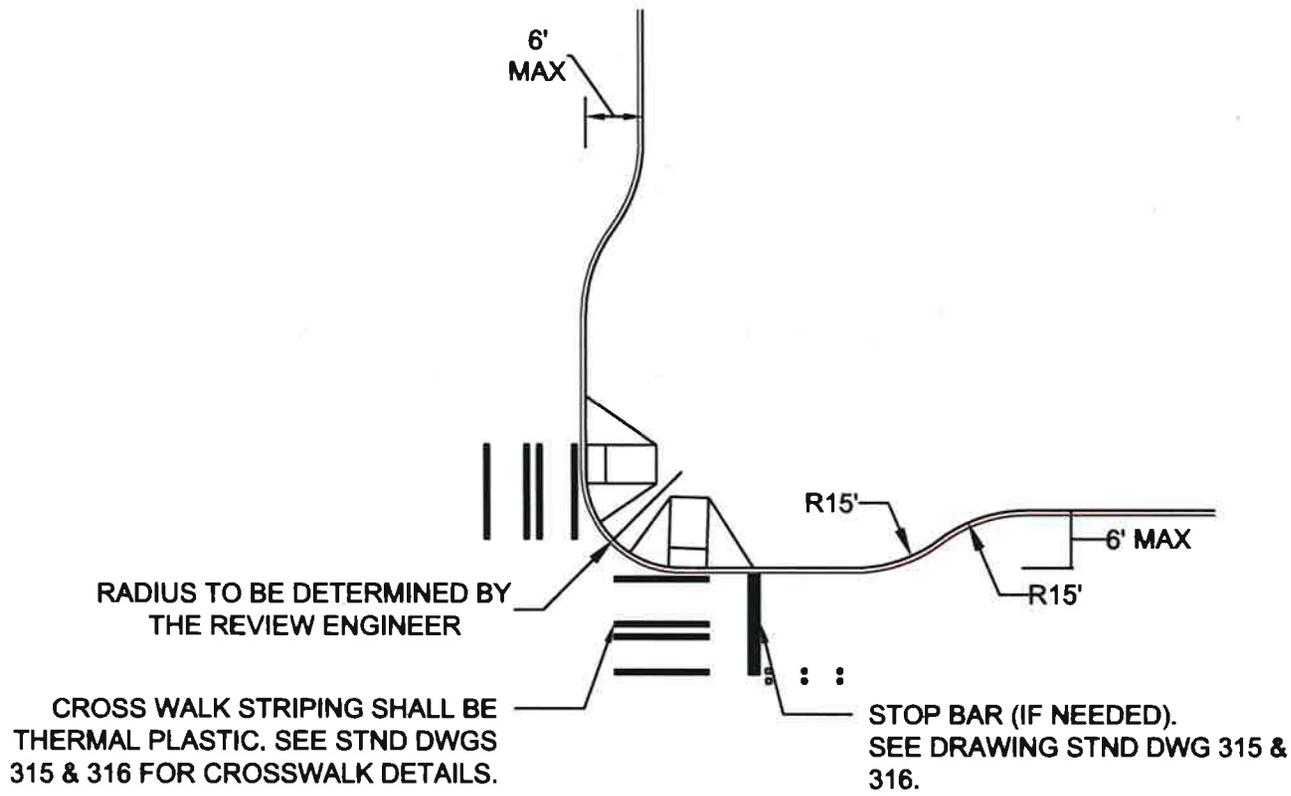


REVERSE DRIVEWAY CURB AND GUTTER (B)

<h1>312</h1> <h2>Curbs</h2>	
 CITY OF SHORELINE Public Works	 PATRICIA J. JENSEN 3570 REGISTERED PROFESSIONAL ENGINEER
NOT TO SCALE	Revision Date April 2017



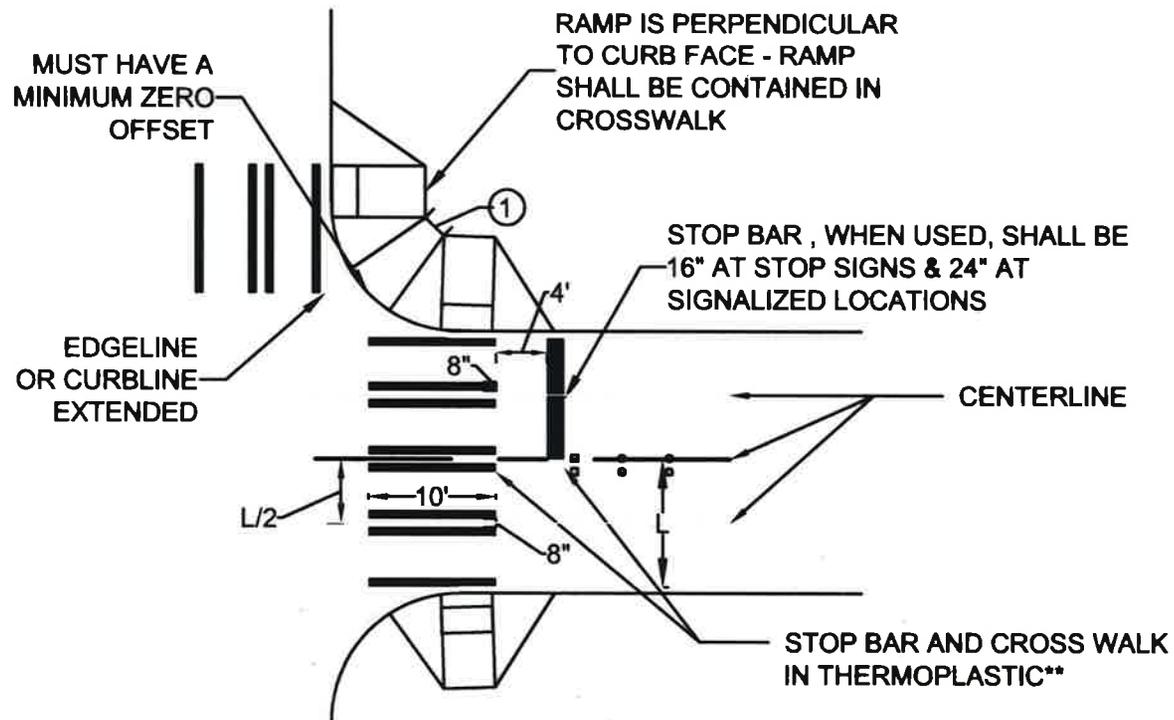
313 Pedestrian Curb	
 CITY OF SHORELINE Public Works	 PATRICIA JURKOVIC STATE OF WASHINGTON 2010 REGISTERED PROFESSIONAL ENGINEER 1917
NOT TO SCALE	Revision Date April 2017



NOTES:

1. INTERSECTION RADII SHALL ACCOMMODATE DESIGN VEHICLES APPLICABLE TO STREET.
2. LENGTH OF CURB EXTENSIONS MUST RECOGNIZE SITE CONDITIONS, E.G. DRIVEWAY LOCATIONS.

<h1 style="margin: 0;">314</h1> <h2 style="margin: 0;">Curb Extension</h2>	
 City of SHORELINE Public Works	 <i>Tania J. Jurek</i>
NOT TO SCALE	Revision Date April 2017



* USE 5' TYPICAL SPACING IF LANES LINES ARE NOT MARKED

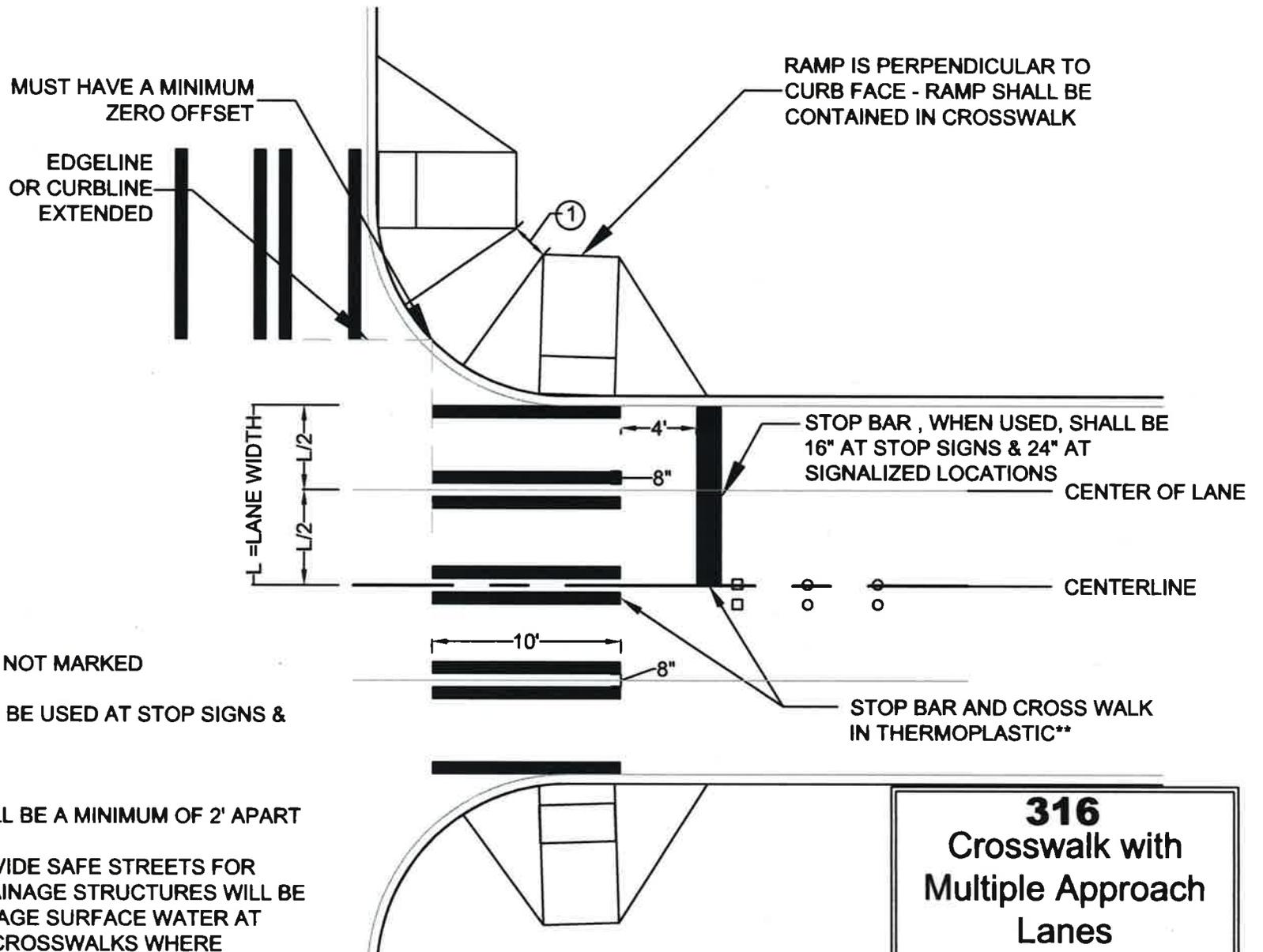
** STOP BAR ONLY TO BE USED AT STOP SIGNS & SIGNALS

NOTES:

①. CURB RAMPS SHALL BE A MINIMUM OF 2' APART 2.

2. IN ORDER TO PROVIDE SAFE STREETS FOR PEDESTRIANS, DRAINAGE STRUCTURES WILL BE REQUIRED TO MANAGE SURFACE WATER AT MARKED CROSSWALKS WHERE AFFECTED BY RUNOFF. SHEET FLOW IS ACCEPTABLE AT UNMARKED CROSSWALKS.

<h1>315</h1> <h2>Crosswalk with Single Approach Lane</h2>	
 City of SHORELINE Public Works	 PATRICIA JENNINGS PROFESSIONAL ENGINEER
NOT TO SCALE	Revision Date April 2017



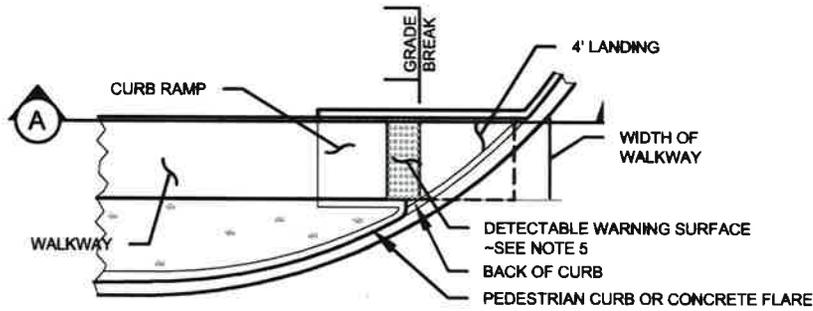
* IF LANES LINES ARE NOT MARKED

** STOP BAR ONLY TO BE USED AT STOP SIGNS & SIGNALS

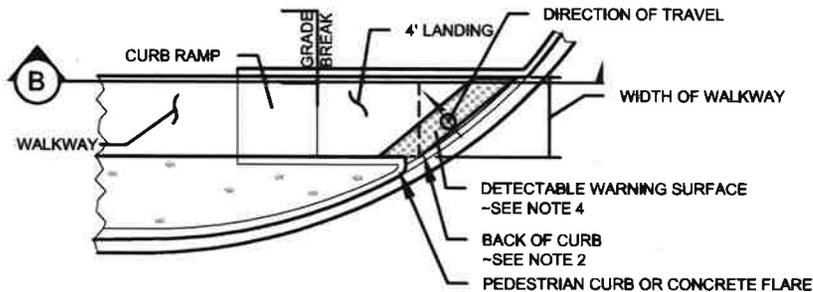
NOTES:

- ① CURB RAMP SHALL BE A MINIMUM OF 2' APART
- 2. IN ORDER TO PROVIDE SAFE STREETS FOR PEDESTRIANS, DRAINAGE STRUCTURES WILL BE REQUIRED TO MANAGE SURFACE WATER AT PAINTED/MARKED CROSSWALKS WHERE AFFECTED BY RUNOFF. SHEET FLOW IS ACCEPTABLE AT UNMARKED CROSSWALKS.

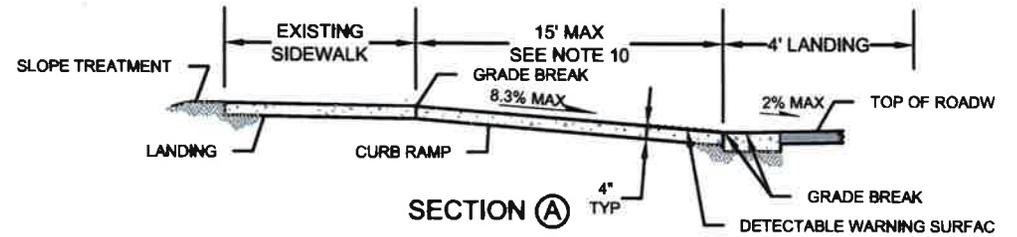
<h2 style="margin: 0;">316</h2> <h3 style="margin: 0;">Crosswalk with Multiple Approach Lanes</h3>	
 CITY OF SHORELINE Public Works	 PATRICIA JARAMA CIVIL ENGINEER No. 11191
NOT TO SCALE	Revision Date April 2017



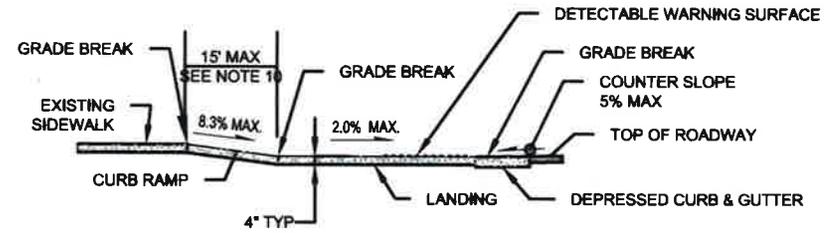
LANDING ≤ 5 FT. FROM BACK OF CURB
(SEE NOTE 7)



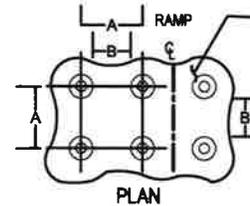
LANDING > 5 FT. FROM BACK OF CURB
(SEE NOTE 7)



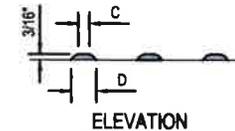
SECTION A



SECTION B



DETECTABLE WARNING PATTERN (TRUNCATED DOMES) SHALL BE YELLOW UNLESS APPROVED BY THE DIRECTOR OR DESIGNEE



	MIN.	MAX.
A	1 5/8"	2 3/8"
B	5/8"	1 1/2"
C	7/16"	3/4"
D	7/8"	1 1/16"

NOTES:

1. THE DETECTABLE WARNING SURFACE (DWS) SHALL EXTEND THE FULL WIDTH OF THE CURB RAMP (EXCLUSIVE OF FLARES) OR THE LANDING.
2. THE DETECTABLE WARNING SURFACE SHALL BE PLACED AT THE BACK OF CURB, AND NEED NOT FOLLOW THE RADIUS.
3. DETECTABLE WARNING PATTERN (TRUNCATED DOMES) SHALL BE FLUSH WITH RAMP TEXTURE +/- NO GREATER THAN 1/4".
4. THE ROWS OF TRUNCATED DOMES SHALL BE ALIGNED TO BE PERPENDICULAR TO THE GRADE BREAK AT THE BACK OF CURB.
5. THE ROWS OF TRUNCATED DOMES SHALL BE ALIGNED TO BE PARALLEL TO THE DIRECTION OF TRAVEL.
6. IF CURB AND GUTTER ARE NOT PRESENT, SUCH AS A SHARED-USE PATH CONNECTION, THE DETECTABLE WARNING SURFACE SHALL BE PLACED AT THE PAVEMENT EDGE.
7. SEE STANDARD PLANS FOR SIDEWALK AND CURB RAMP DETAILS.
8. IF A CURB RAMP IS REQUIRED, THE LOCATION OF THE DETECTABLE WARNING SURFACE MUST BE AT THE BOTTOM OF THE RAMP AND WITHIN THE REQUIRED DISTANCE FROM THE RAIL.
9. WHEN THE GRADE BREAK BETWEEN THE CURB RAMP AND THE LANDING IS LESS THAN OR EQUAL TO 5 FT. FROM THE BACK OF CURB AT ALL POINTS, PLACE THE DETECTABLE WARNING SURFACE ON THE BOTTOM OF THE CURB RAMP.
10. THE CURB RAMP MAXIMUM RUNNING SLOPE SHALL NOT REQUIRE THE RAMP LENGTH TO EXCEED 15 FEET TO AVOID CHASING THE SLOPE INDEFINITELY WHEN CONNECTING TO STEEP GRADES. WHEN APPLYING THE 15 FOOT MAXIMUM LENGTH, THE RUNNING SLOPE OF THE CURB RAMP SHALL BE AS FLAT AS FEASIBLE.
11. WHERE "GRAD BREAK" IS CALLED OUT, THE ENTIRE LENGTH OF THE GRADE BREAK BETWEEN THE TWO ADJACENT SURFACE PLANES SHALL BE FLUSH.
12. DO NOT PLACE GRATINGS, JUNCTION BOXES, ACCESS COVERS, OR OTHER APPURTENANCES INFRONT OF THE CURB RAMP OR ANY PART OF THE CURB RAMP OR LANDING.
13. CURB RAMP, LANDING, AND FLARES SHALL RECEIVE BROOM FINISH.

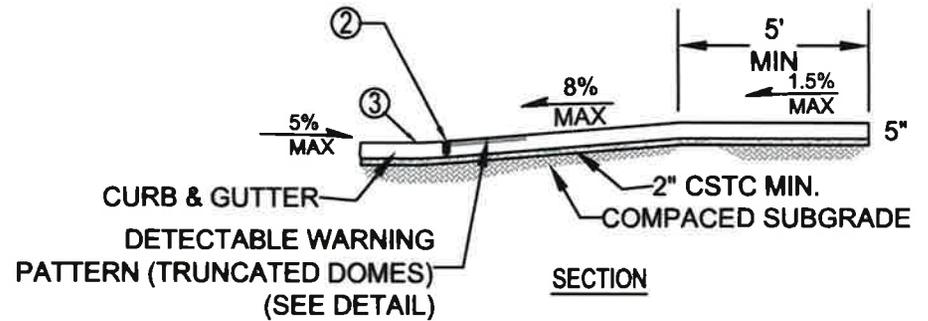
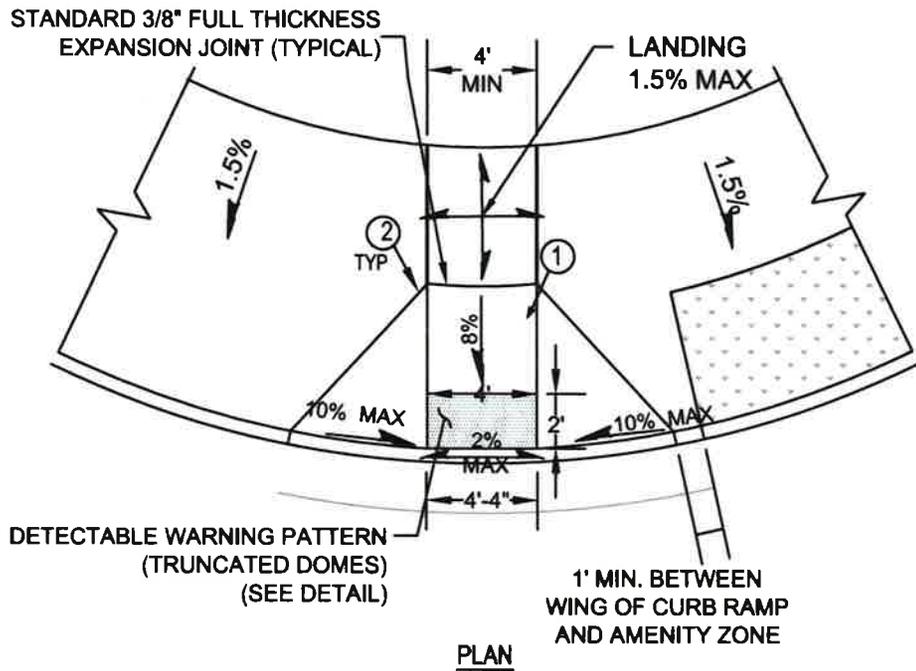
DETECTABLE WARNING PATTERN DETAIL
TRUNCATED DOMES
(SEE NOTE 3)

317

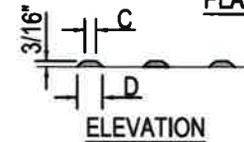
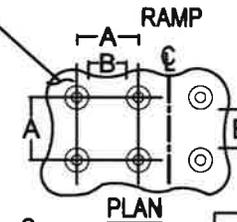
Single Direction Curb Ramp

Public Works

NOT TO SCALE
Revision Date
April 2017



DETECTABLE WARNING PATTERN (TRUNCATED DOMES) SHALL BE YELLOW UNLESS APPROVED BY THE DIRECTOR OR DESIGNEE



	MIN.	MAX.
A	1 5/8"	2 3/8"
B	5/8"	1 1/2"
C	7/16"	3/4"
D	7/8"	1 7/16"

DETECTABLE WARNING PATTERN DETAIL TRUNCATED DOMES (SEE NOTE 6)

NOTES:

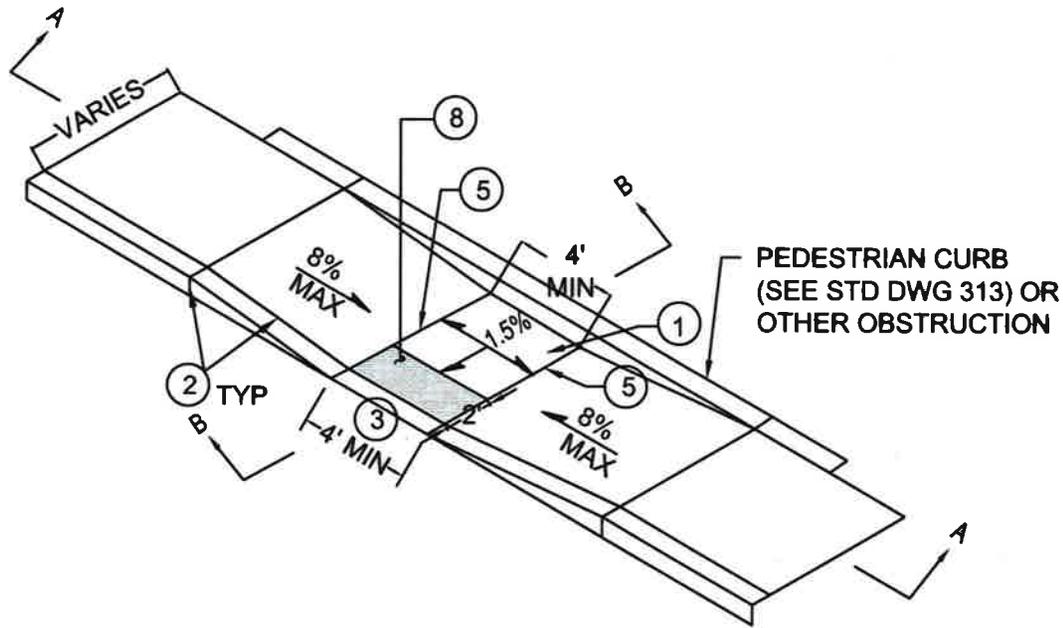
- ① BROOM FINISH PERPENDICULAR TO THE DIRECTION OF PEDESTRIAN TRAVEL
2. STANDARD 3/8" FULL THICKNESS EXPANSION JOINT (TYPICAL). FULL DEPTH EXPANSION JOINT
- ③ NO LIP AT GUTTER LINE. CURB SHALL BE FLUSH AT GUTTER LINE.
4. CURB RAMPS WILL BE POURED INTEGRAL WITH SIDEWALK AND SHALL BE ISOLATED BY EXPANSION JOINT MATERIAL ON ALL SIDES, BUT NOT AT END OF RAMP ADJACENT TO THE ROADWAY.
5. CATCH BASINS & INLETS SHALL BE INSTALLED A MINIMUM OF 12" FROM THE BASE OF CURB RAMP.
6. DETECTABLE WARNING PATTERN (TRUNCATED DOMES) SHALL BE FLUSH WITH RAMP TEXTURE +/- NO GREATER THAN 1/4".
7. CONCRETE SHALL BE A MINIMUM OF 5" THICK.
8. CURB RAMPS MUST MEET CURRENT ADA REQUIREMENTS.
9. MINIMUM OF 2' WITH ADJACENT RAMPS.
10. RAMP AND LANDING WIDTHS SHALL NOT INCLUDE EXPANSION JOINTS.

318
Curb Ramp: Type Perpendicular

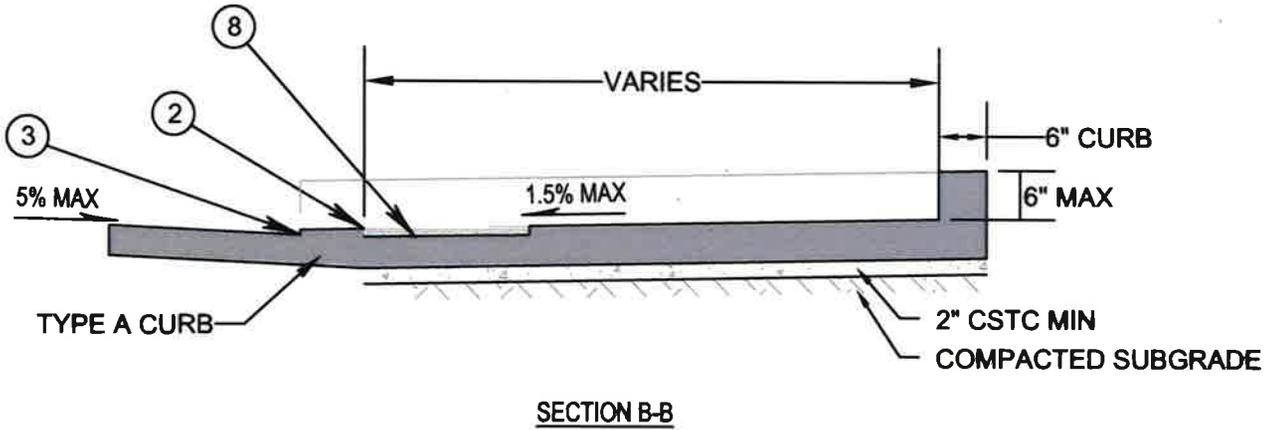
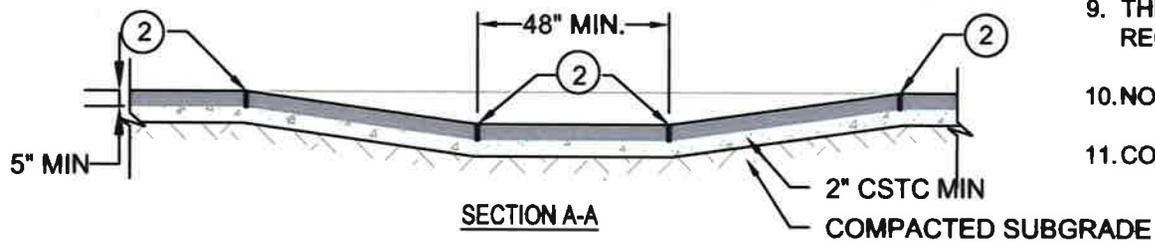
Public Works

NOT TO SCALE

Revision Date
April 2017



- NOTES:**
- ① BROOM FINISH PERPENDICULAR TO THE DIRECTION OF PEDESTRIAN TRAVEL
 - ② $\frac{3}{8}$ " EXPANSION JOINT (TYP). FULL DEPTH EXPANSION JOINT.
 - ③ NO LIP AT GUTTER LINE. CURB SHALL BE FLUSH AT GUTTER LINE.
 4. CURB RAMPS SHALL BE ISOLATED BY EXPANSION JOINT MATERIAL ON ALL SIDES
 - ⑤ $\frac{3}{4}$ " RADIUS DUMMY JOINT
 6. CATCH BASINS & INLETS SHALL BE INSTALLED A MINIMUM OF 12" FROM THE BASE OF CURB RAMP LANDING.
 7. DETECTABLE WARNING PATTERN (TRUNCATED DOMES) SHALL BE FLUSH WITH RAMP TEXTURE +/- NO GREATER THAN $\frac{1}{4}$ ".
 - ⑧ SEE STND DWG 318 FOR DETECTABLE WARNING PATTERN (TRUNCATED DOMES)
 9. THE CURB RAMP MUST MEET CURRENT ADA REQUIREMENTS.
 10. NO LAMPBLACK SHALL BE PLACED IN CURB RAMPS.
 11. CONCRETE SHALL BE A MINIMUM OF 5" THICK.

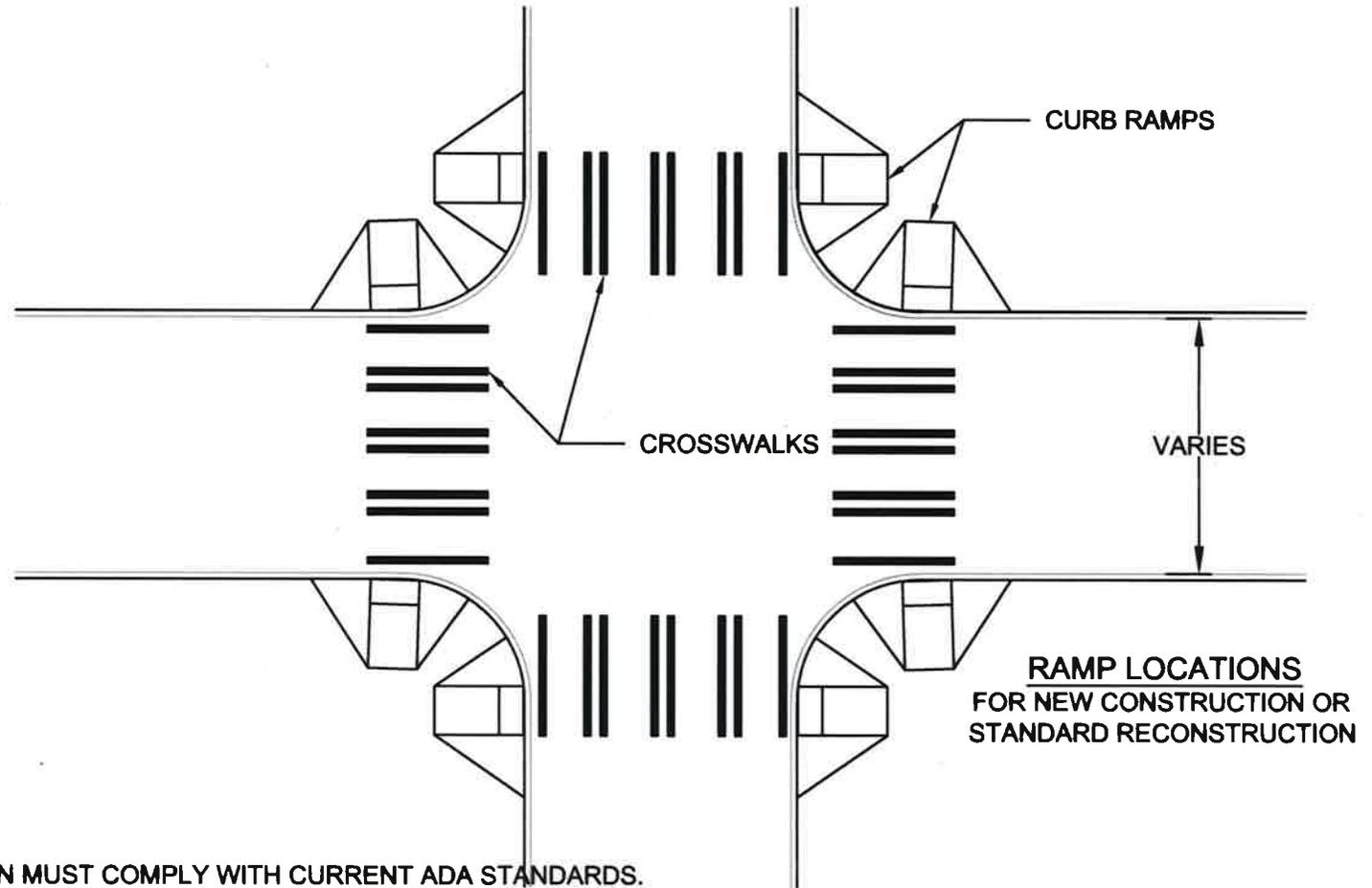


320
Curb Ramp: Type Parallel

Public Works

NOT TO SCALE

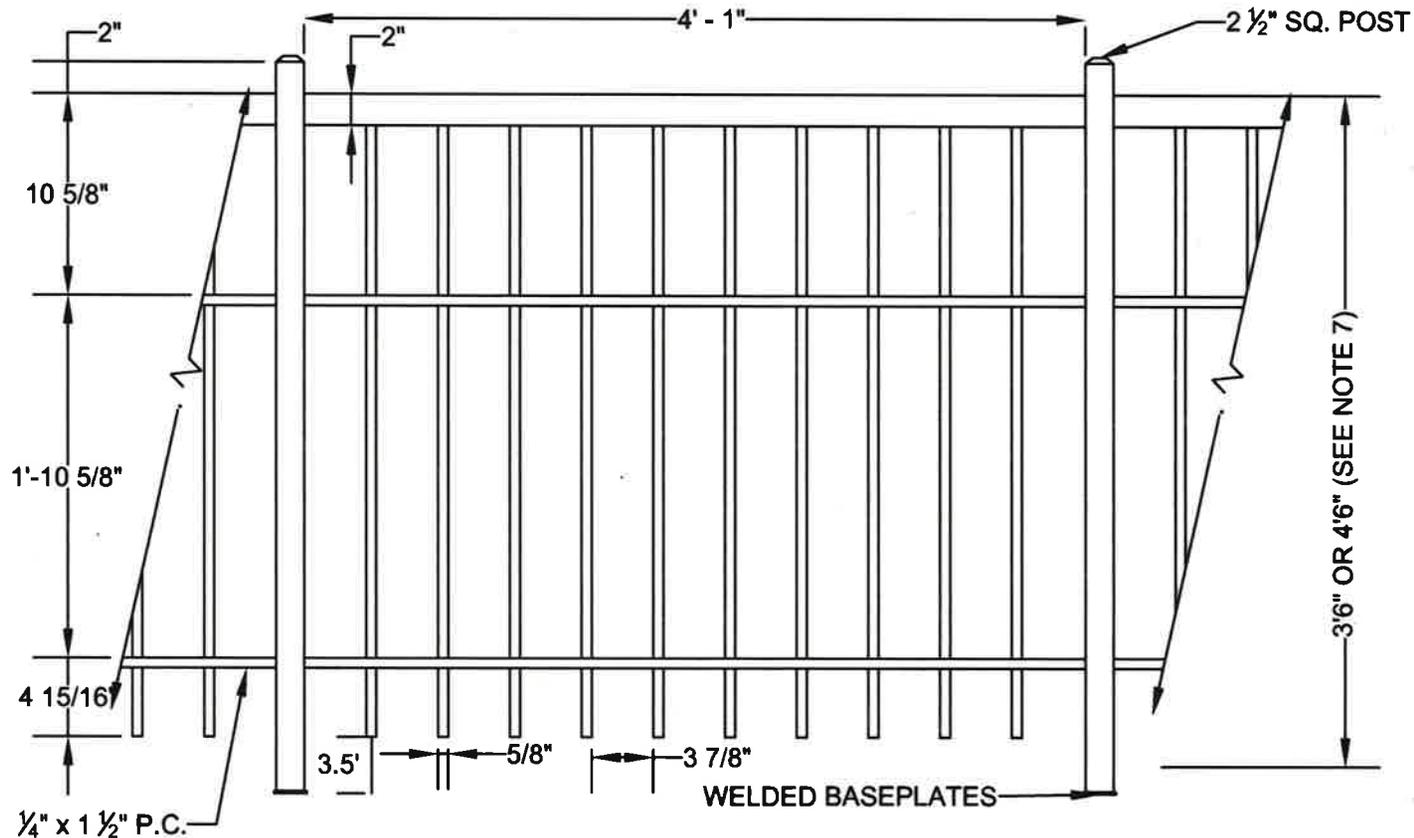
Revision Date
April 2017



NOTES:

1. CURB RAMP CONSTRUCTION MUST COMPLY WITH CURRENT ADA STANDARDS.
2. CONSTRUCT RAMP WITH A MINIMUM 1' CLEARANCE FROM FIXED OBJECTS SUCH AS HYDRANTS, POLES, INLETS, AND OTHER UTILITIES.
3. CONSTRUCT RAMP IN ACCORDANCE WITH STND DWGS 318 & 320.
4. CROSSWALKS ARE NOT ALWAYS MARKED.
5. WHEN RAMPS ARE CONSTRUCTED ON ONE SIDE OF STREET, RAMPS SHALL BE CONSTRUCTED AT CORRESPONDING LOCATIONS ON OPPOSITE SIDE OF STREET.
6. CROSSWALK SHALL INTERSECT AT THE CURB OR BEYOND - NOT IN THE STREET AREA.
7. IN ORDER TO PROVIDE SAFE STREETS FOR PEDESTRIANS, DRAINAGE STRUCTURES WILL BE REQUIRED TO MANAGE SURFACE WATER AT PAINTED/MARKED CROSSWALKS WHERE AFFECTED BY RUNOFF. SHEET FLOW IS ACCEPTABLE AT UNMARKED CROSSWALKS.

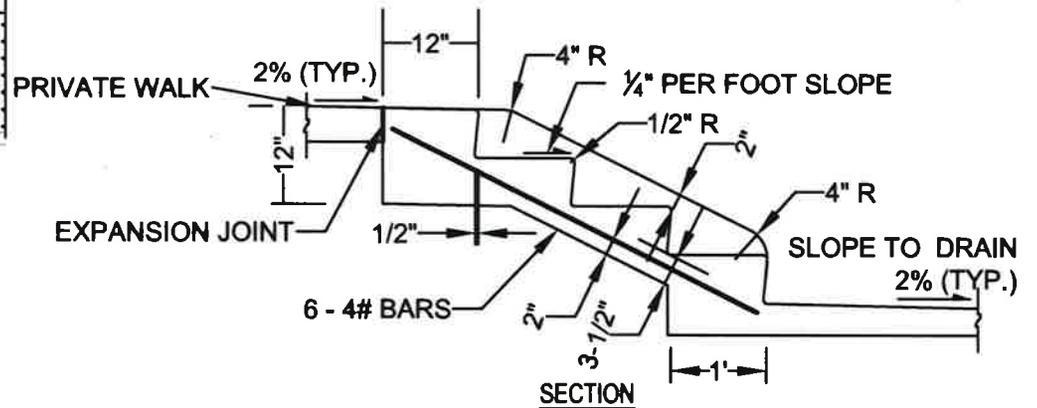
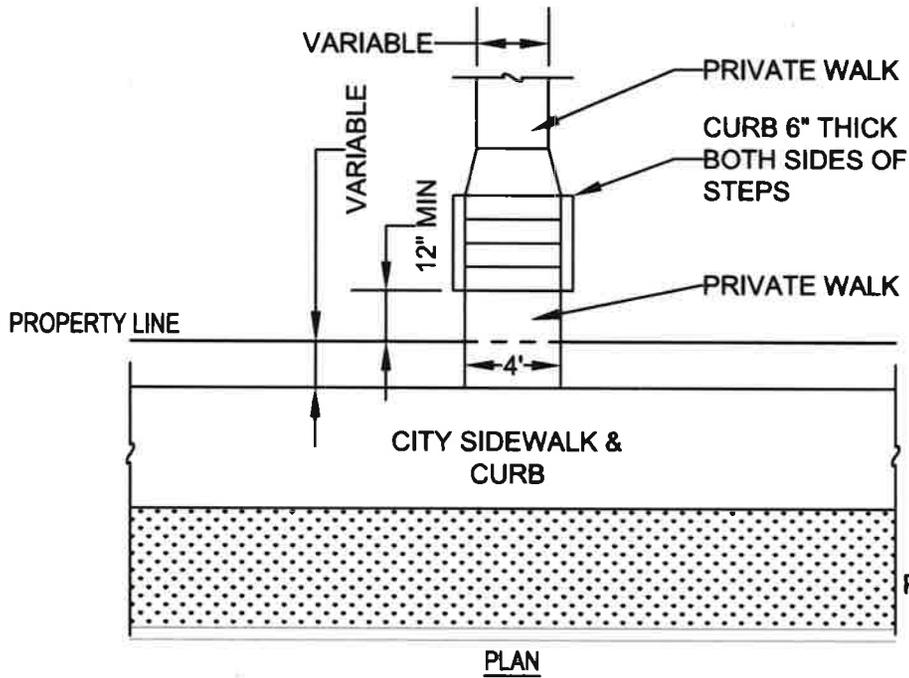
<h2 style="margin: 0;">321 Curb Ramp Locations</h2>	
 CITY OF SHORELINE Public Works	 TRICIA JURINA PROFESSIONAL ENGINEER 3000 4/19/17
NOT TO SCALE	Revision Date April 2017



NOTES:

1. SHOP DRAWINGS OF RAILING SHALL BE SUBMITTED FOR APPROVAL SHOWING COMPLETE DIMENSIONS AND DETAILS OF FABRICATION AND INCLUDING AN ERECTION DIAGRAM. MATERIALS BEING USED SHALL BE SPECIFIED IN THE SHOP DRAWINGS.
2. ALL ALUMINUM PARTS SHALL BE GIVEN A CLEAR ANODIC COATING AT LEAST 0.0006 INCH THICK AND BE HOT WATER SEALED AND SHALL HAVE A UNIFORM FINISH.
3. CUTTING SHALL BE DONE BY SAWING OR MILLING AND ALL CUTS SHALL BE TRUE AND SMOOTH. FLAME CUTTING WILL NOT BE PERMITTED.
4. PIPE RAILING, PIPE BALUSTERS AND PIPE RAILING SPLICES SHALL BE ADEQUATELY WRAPPED TO ENSURE SURFACE PROTECTION DURING HANDLING AND TRANSPORTATION TO THE JOB SITE.
5. WELDING OF ALUMINUM SHALL BE IN ACCORDANCE WITH THE LATEST AASHTO STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS.
6. ALLOW FOR EXPANSION AT APPROXIMATELY EVERY FOURTH POST.
7. TOP OF RAIL:
 - 3 FEET 6 INCHES FOR PEDESTRIAN USES
 - 4 FEET 6 INCHES FOR COMBINED BICYCLE AND PEDESTRIAN USES
8. 50LB RAIL LOADING PER IBC, CURRENT EDITION.
9. ALL CONCEALED FASTENERS THROUGHOUT.
10. MILD STEEL.
11. BLACK POWDER COATED.

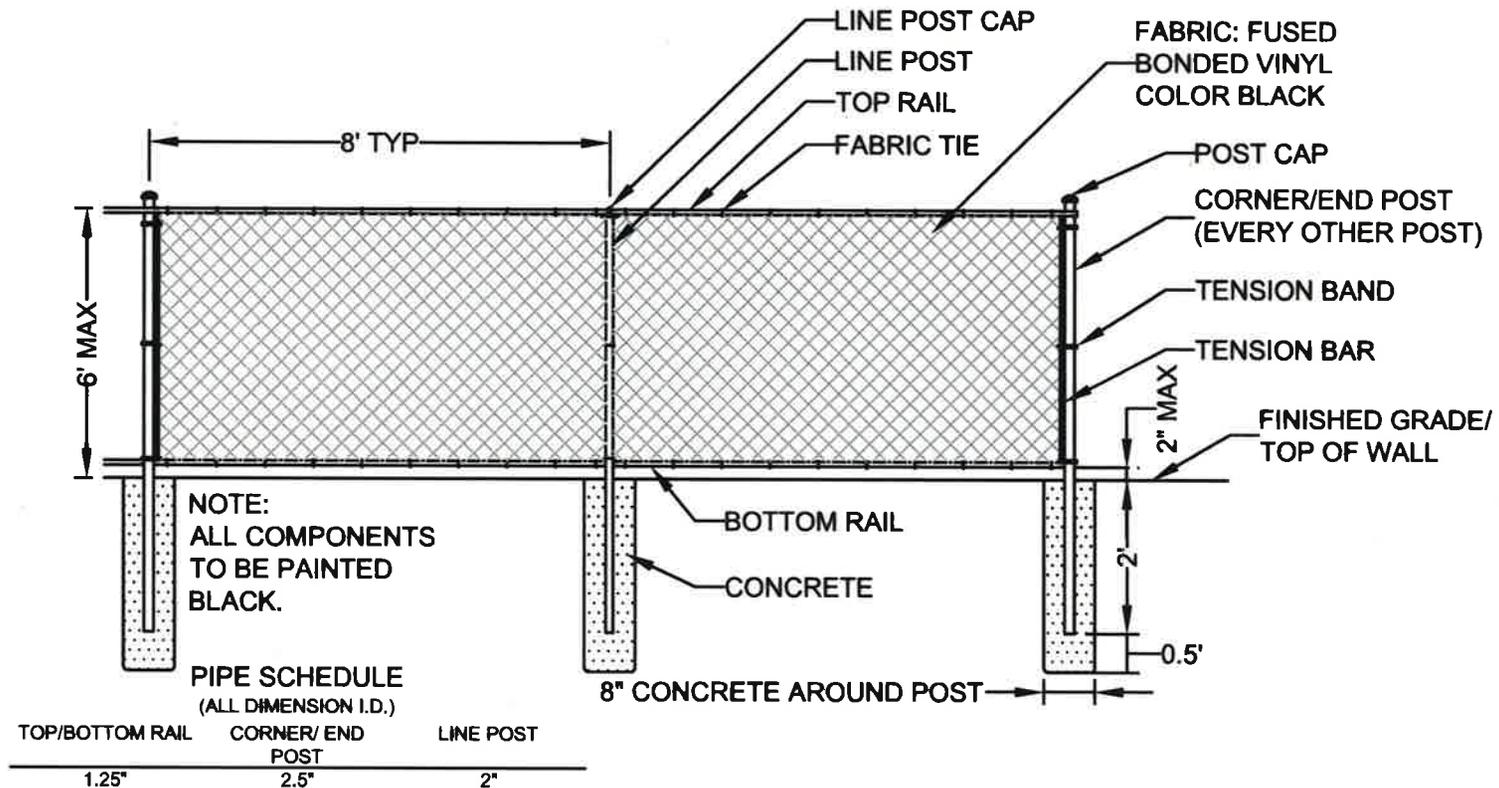
<h1 style="margin: 0;">324</h1> <h2 style="margin: 0;">Pedestrian Railing</h2>	
 CITY OF SHORELINE Public Works	 <i>Patricia J. Jernigan</i>
NOT TO SCALE	Revision Date April 2017



NOTES:

1. STEPS SHALL BE A MINIMUM OF 4'-0" WIDE, CURB TO CURB, PLUS 6" CURBS ON EACH SIDE.
2. CEMENT CONCRETE SHALL BE CLASS 4000psi TROWEL FINISH.
3. NUMBER OF STEPS SHALL SUIT INDIVIDUAL CONDITIONS, WITH TREAD AND RISER DIMENSIONS TO SUIT THE GRADE.
4. RISERS SHALL BE 5" MINIMUM, 7" MAXIMUM: TREAD SHALL BE 11" MINIMUM, 12" MAXIMUM.
5. HANDRAIL REQUIRED ON BOTH SIDES PER IBC.

<h2 style="margin: 0;">325 Stairs</h2>	
<p>Public Works</p>	
<p>NOT TO SCALE</p>	<p>Revision Date April 2017</p>



NOTES:

1. RAILING SHALL BE ALUMINUM PIPE RAIL OR APPROVED EQUIVALENT. INSTALLATION PER MANUFACTURER'S RECOMMENDATIONS.
2. SHOP DRAWINGS OF RAILING SHALL BE SUBMITTED FOR APPROVAL SHOWING COMPLETE DIMENSIONS AND DETAILS OF FABRICATION AND INCLUDING AN ERECTION DIAGRAM. MATERIALS BEING USED SHALL BE SPECIFIED IN THE SHOP DRAWINGS.
3. ALL ALUMINUM PARTS SHALL BE GIVEN A BLACK ANODIC COATING AT LEAST 0.0006 INCH THICK AND BE HOT WATER SEALED AND SHALL HAVE A UNIFORM FINISH.
4. WIRE FABRIC SHALL BE GIVEN A BLACK FUSED BONDED VINYL COATING TO MATCH FINISHED POSTS.
5. CUTTING SHALL BE DONE BY SAWING OR MILLING AND ALL CUTS SHALL BE TRUE AND SMOOTH. FLAME CUTTING WILL NOT BE PERMITTED.
6. ALL MATERIALS SHALL BE ADEQUATELY WRAPPED TO ENSURE SURFACE PROTECTION DURING HANDLING AND TRANSPORTATION TO THE JOB SITE.
7. ANY WELDING OF ALUMINUM SHALL BE IN ACCORDANCE WITH THE LATEST AASHTO STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS.
8. RAILS, POSTS AND FORMED ELBOWS SHALL BE A.S.T.M B-241 OR B-429 ALLOY, 6063-T6 SCHEDULE 40 (STD. PIPE). BRACKETS, ENDCAPS AND OTHER FITTINGS SHALL BE A.S.T.M. 6063-T5. SPLICES AND REINFORCING SLEEVES SHALL BE DRAWN ALUMINUM TUBING 6063-T832.
9. TOP OF RAIL: 3 FEET 6 INCHES MIN FOR PEDESTRIAN USES
4 FEET 6 INCHES MIN FOR COMBINED BICYCLE AND PEDESTRIAN USES

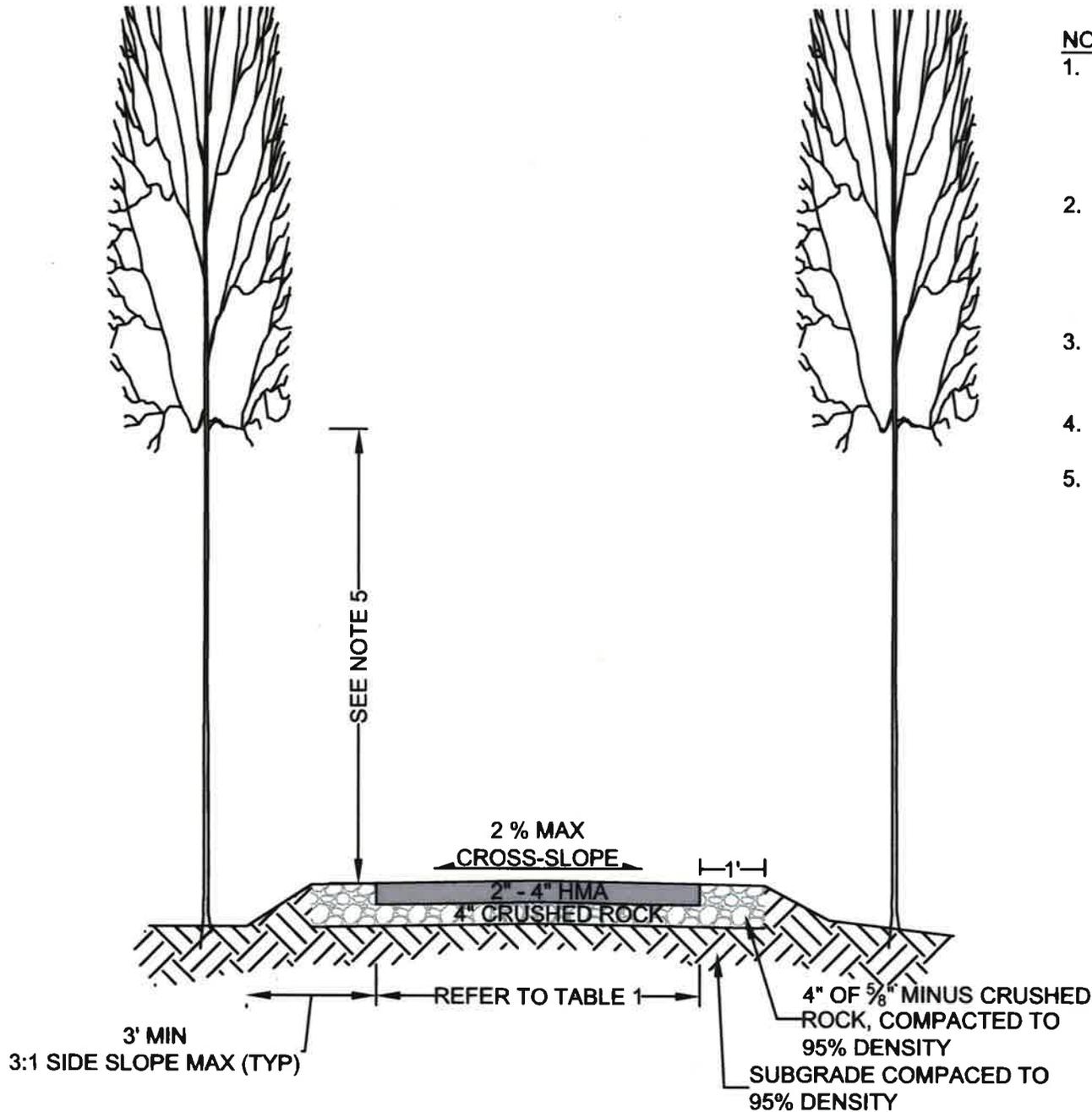
327
Chain Link Fencing




Public Works

NOT TO SCALE

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NOTES:

1. ALL PLANS MUST BE APPROVED BY THE CITY PRIOR TO CONSTRUCTION OF THE TRAIL. TRAIL CENTERLINE TO BE STAKED IN FIELD BY CONTRACTOR AND APPROVED BY THE APPROPRIATE CITY INSPECTOR.
2. ALL HAZARD TREES AND TREE LIMBS, AS DEFINED BY THE WASHINGTON STATE DEPARTMENT OF NATURAL RESOURCES HAZARD TREE BULLETIN, SHALL BE FELLED AND REMOVED FROM THE SITE.
3. SUBGRADE TO BE TREATED WITH AN APPROVED HERBICIDE PRIOR TO PLACING ASPHALT.
4. MINIMUM BRANCH CLEARANCE ABOVE TRAIL SURFACE = 7'-0" (TYPICAL).
5. ANY TRAILS USED FOR MAINTENANCE, 8' MIN.

USE	WIDTH
FOOT PATH	5'
BIKE PATH (ONE WAY)	8'
BIKE PATH (TWO WAY)	12'

329 Typical Sections for Trails



CITY OF SHORELINE
Public Works



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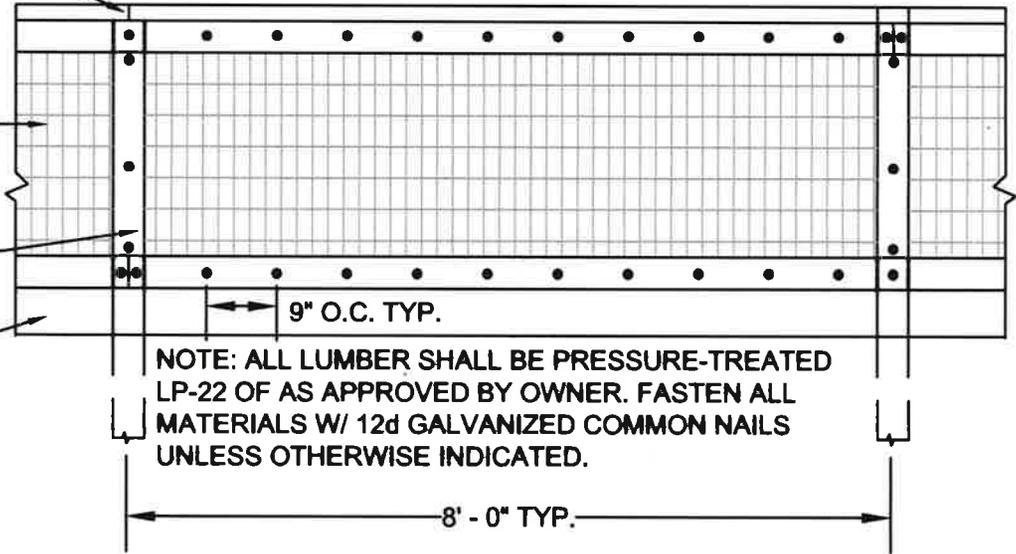
ALTERNATE JOINTS W/
TOP 2X4 RAIL

(2) 2X4 HORIZ. RAILS
SANDWICHED

FENCE FABRIC (CONT.) 2X4, 10GA/
10GA WOVEN FABRIC, HOT DIP
GALVANIZED FINISH

VERT. 2X4 @ POST
(4X4 POST BEYOND)

(2) 2X4 HORIZ.
RAILS SANDWICHED



ELEVATION

2X6 CAP, EASE ALL
EXPOSED EDGES 1/4"

(2) 2X4, INSTALL W/ 3/8"
CARRIAGE BOLTS (TYP.)

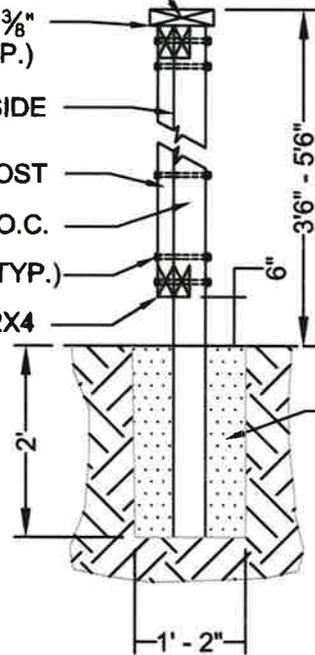
FENCE FABRIC FACING SIDE

VERTICAL 2X4 AT EA. POST

4X4 POST @ 4'6" O.C.

GRIND BOLTS FLUSH W/ HEX NUT (TYP.)

(2) 2X4



CONCRETE BACK FILL

SECTION

NOTE: DIAGONAL BRACE RAILS (4x4) TO BE INSTALLED AT ALL END SECTIONS, TOP OF BRACE AT TOP OF END SECTION/CORNER.

335 Greenbelt Fence



Public Works

NOT TO SCALE



Revision Date
April 2017